

STATEMENT OF ENVIRONMENTAL EFFECTS

CONSTRUCTION OF A HEAVY DUTY DRIVEWAY AND EXTENSION OF EXISTING STAFF CARPARK

81 & 75 GREENACRE ROAD, GREENACRE

PREPARED FOR AL NOORI MUSLIM SCHOOL
PB232024

Date	Version	Prepared By
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12 August 2024	Final SEE	Julie Horder - Director

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1 INTRODUCTION

1.1 Overview

This Statement of Environmental Effects (SEE) has been prepared by Planning Bricks on behalf of Al Noori Muslim School (the Applicant) to accompany a Development Application (DA) lodged pursuant to Section 4.12 of the Environmental Planning and Assessment Act 1979. The DA seeks consent for the construction of a heavy duty driveway and a staff car park for use as part of the Al Noori Muslim School.

The Al Noori Muslim primary school campus is located to the east of the subject site, on the north western corner of Greenacre Road and Noble Avenue and the Al Noori Muslim secondary school campus is located to the west of the subject site with frontages to Greenacre Road and Mimosa Road.

This SEE is structured as follows:

- Section 1 – provides an introduction and background to the application;
- Section 2 – provides a detailed description of the site and its locality;
- Section 3 - provides a detailed description of the proposed development;
- Section 4 – provides an assessment of the proposed development against the relevant planning provisions;
- Section 5 – provides an assessment of the potential environmental and social impacts and sets outs any proposed mitigation measures; and
- Section 6 – provides a conclusion.

1.2 Supporting Documentation

This DA is supported by the following reports and plans:

- Architectural Plans prepared by Millennium Design Consultants Pty Ltd;
- Stormwater Concept Plans prepared by Loka Consulting Engineers Pty Ltd;
- Survey Plan prepared by XY Positioning Pty Ltd;
- Landscape Plans prepared by A Total Concept Landscape Architects; and
- Letter of Advice regarding Proposed Right of Way prepared by TTPA.

1.3 Background to Al Noori Muslim School

Al Noori Muslim School is an independent, co-educational school for Kindergarten to Year 12 students.

The school was originally established in 1983, in Lakemba. As the school grew, they relocated 9 times until finding their current premises in 1987 which catered for primary school children. In 2009, the first Year 7 cohort was inducted and in 2013 a new high school facility was opened. The school has continued to grow and has slowly redeveloped underutilised areas of the existing site to create increased opportunities for new buildings, outdoor play areas and parking. A short planning history of the Al Noori Muslim school is provided in the table below.

Table 1 Planning History – Al Noori Muslim School

Ref No.	Description of Approval	Date of Determination
DA-593/2005	Replacement of four (4) existing demountable classrooms and construction of a new two (2) storey classroom building consisting of four (4) new classrooms and one (1) staffroom	11 October 2005
DA 1016/2007	Conversion of an existing dwelling house to a school for 60 Students from Grades Three (3) to Six (6) at No.93 Greenacre Road	Approved in the NSW Land and Environment Court, in October 2008
DA No.128/2009	The consent was for the demolition of the existing structures, removal of trees and construction of a secondary educational facility for 310 students including an indoor multi-purpose centre, library, administrative offices, car parking, internal road works, landscaping, and boundary fencing	14 July 2009 This consent was never acted upon by the school.
DA-838/2012	The use of the site for a secondary school - 310 secondary male and female students from years 7 to 12. Included demolition of the existing demountable buildings located towards the Mimosa Road frontage, and one dwelling located at the Greenacre Road frontage, the use of the existing 3 storey school building for school purposes, and the finalisation of car parking, site works and landscaping.	26 February 2013 Deferred commencement
DA-838/2012/1	Modification Application - an increase in the number of secondary students from 310 to 600. Provision of a landscaped area in the northern portion of the site fronting Mimosa Road in the area of the previously approved car park. Retention of the existing tennis courts for a play area. Provision of student,	28 October 2014

Ref No.	Description of Approval	Date of Determination
	teacher, and parent parking in the south eastern portion of the site, in the area of the previously approved landscaped area	
CD-373/2015	Complying Development Certificate - Demolition of existing dwelling and outbuilding (84 Noble Avenue)	30 July 2015
CD-382/2018	Complying Development Certificate - the erection of a three storey school building over basement parking level together with associated services.	15 August 2008 (OC issued 25 June 2021)
CD-839/2019	Complying Development Certificate - Construction of basement and class room Building B (89 Greenacre Rd)	15 November 2019
CD-398/2021	Complying Development Certificate - Erection of a three storey school building over basement parking level together with associated services (89 Greenacre Rd)	14 April 2021
CD-633/2021	Complying Development Certificate - Demolition of 8 single storey residential homes (89 Greenacre Rd)	30 June 2021
DA-109/2022	Change of land use to allow the land previously known as 104 Mimosa Road to be used as a school playground	29 April 2022 (CC Approved 21 July 2022)
CD-1356/2022	Complying Development Certificate - Demolition of existing house structures (62 – 72 Mimosa Road, 82 and 86 Noble Avenue and 75 Greenacre Road, Greenacre).	26 December 2022
DA-1285/2023	Change of use from residential dwellings to school playground and formation of 3 multi-purpose play areas	Approved - 15 May 2024

2 SITE ANALYSIS

2.1 The Site

The subject site consists of 81 Greenacre Road and part of 75 Greenacre Road, Greenacre. The legal description of the site is Lot A in DP 354971 and Lot 1 in DP 1261348.

The subject site is outlined in red on Figure 1 below with the location of the proposed heavy duty driveway and carpark outlined in yellow (the “development zone”).

The development zone has a frontage to Greenacre Road of 26.8m (southern boundary) and a rear (northern) boundary length of 26.8m and side (eastern and western) boundary lengths of 100.58m.

The total area of the development zone is approximately 2695.54m².



Figure 1: Subject site (edged red), development zone (edged yellow) and surrounds (Source: www.nearmaps.com.au)

2.2 Existing Development

The development zone includes a generally vacant area of land at 81 Greenacre Road which was previously owned by Ausgrid. A transmission line stanchion is located in the north western corner of 81 Greenacre Road and power lines traverse over the northern (rear) portion of the site (Figure 2).

The eastern portion of the development zone contains an existing concrete slab utilised for staff parking and a single storey security building fronts Greenacre Road. Whilst this portion of the development zone forms part of the larger Al Noori Muslim primary school site (i.e. 75 Greenacre Road), no works are proposed to the existing primary school buildings. An oblique aerial is provided at Figure 2 below which shows the existing structures and uses within the development zone.



Figure 2: Oblique Aerial - development zone (edged yellow) (Source: www.nearmaps.com.au)

The site slopes from north west to south east. The highest point is at RL53.97 which is located along the north western boundary of the site. From this point, the site slopes to RL51.81 towards the south-eastern corner of the site (along the Greenacre Road frontage).

The development zone is subject to a number of easements including a 5m wide easement for electricity and a 5m wide right of carriageway for Ausgrid access. These easements traverse

the western and northern boundary of the development zone. A Survey Plan of the site is provided under separate cover and an extract is provided at Figure 3 below.

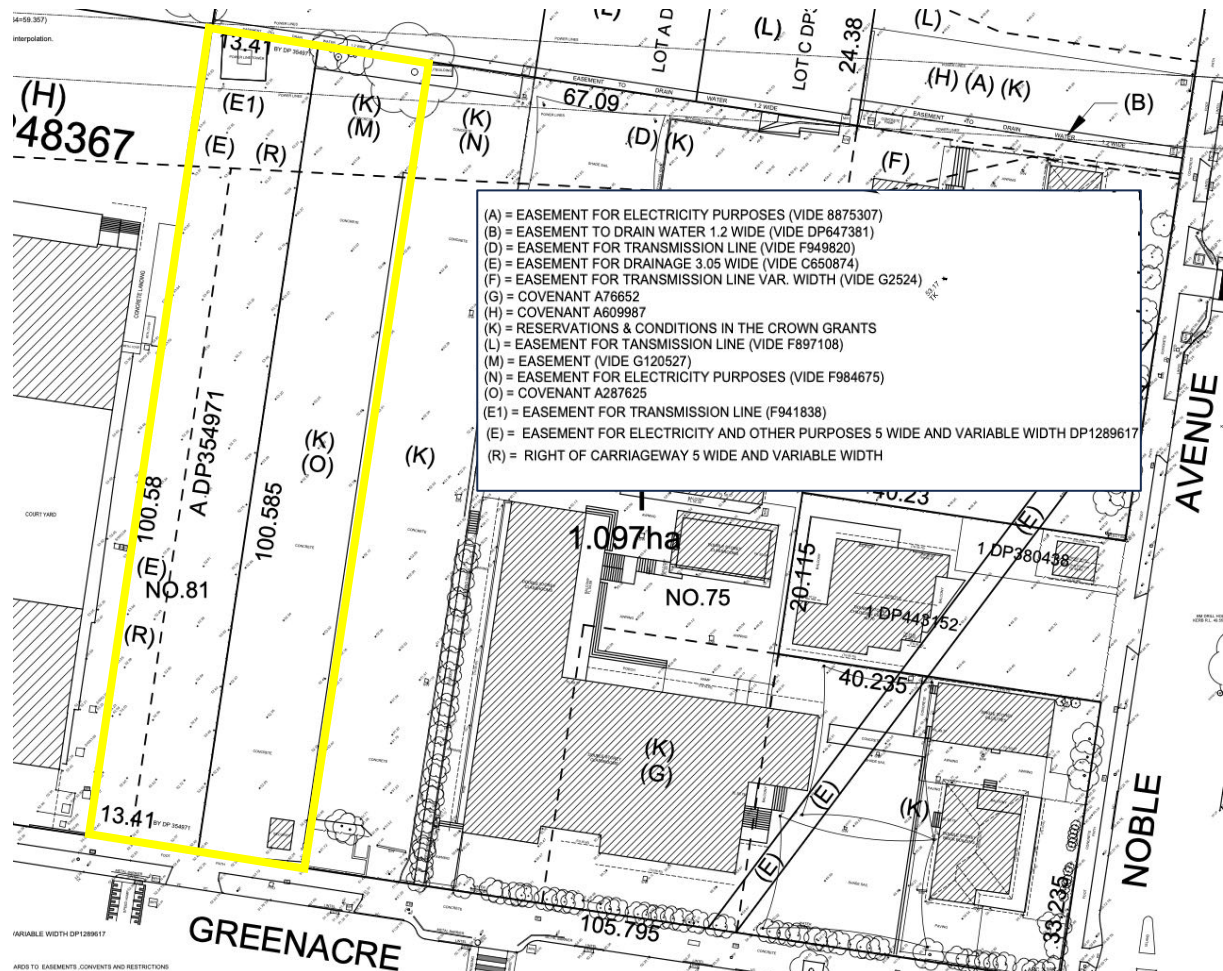


Figure 3: Extract of Survey Plan showing development zone (edged yellow)

2.3 Site Context

2.3.1 Adjoining Development

To the north of the development zone is 68 – 72 Mimosa Road which is currently vacant land forming part of a larger parcel of land owned by the Al Noori Muslim primary school and which has been approved for use as a school playground as part of DA-1285/2023

To the east of the development zone is an existing concrete slab utilised for staff parking and also forms part of 75 Greenacre Road. Beyond the concrete slab is the buildings and play areas of the Al Noori Muslim primary school which is located on the corner of Greenacre Road and Noble Avenue.

To the south of the development zone is Greenacre Road, a local road with a carriageway width of approximately 10m. Greenacre Road provides the main pedestrian entrance to the Al Noori

Muslim primary school and vehicle access is provided off Greenacre Road into the development site. The southern side of Greenacre Road is characterised by one and two storey dwellings.

To the west is the Al Noori Muslim secondary school campus which has frontages to both Greenacre Road and Mimosa Road.

2.3.2 Land Uses Surrounding the Site

Surrounding land uses are primarily residential, consisting of one and two storey detached dwellings. There are also a scattering of supporting land uses including child care centres along Mimosa Road and Noble Avenue and a local shop at 68 Noble Avenue (to the south of the subject site).

2.4 Public Transport

The subject site is well serviced by public transport with several bus stops located along Noble Avenue. Route 939 travels along Noble Avenue, to and from Bankstown town centre. A number of other bus services (946, 941) also travel along Hillcrest Avenue to the west of the site. Bankstown train station is also located 2.5km to the south west of the site.

3 THE PROPOSAL

3.1 Description

Development consent is sought for construction of a heavy duty driveway and concrete carpark and use of land by Al Noori Muslim school for staff car parking.

The land the subject of the works and use (the development zone) was previously utilised for residential purposes with demolition of the dwellings completed in mid 2014.

The proposed car park will provide an additional 36 parking spaces so that a total of 64 staff parking spaces (including 1 accessible parking space) is provided within the existing and proposed car park. A footpath is provided between the parking spaces to enable safe and direct access from the car park to Greenacre Road. The car parking spaces will be utilised by staff of the primary and secondary school of Al Noori Muslim school.

The heavy duty driveway is required to provide new site access for Ausgrid to service and maintain their tower. As detailed in Section 2.2 above, a Right of Way (ROW) has been established along the western boundary of 81 Greenacre Road which comprises the location of the proposed heavy duty driveway. The driveway and circulation space has been designed to accommodate the access movements of a 10.3m long single unit Ausgrid maintenance truck. Sufficient area is provided to permit a vehicle to enter the site forwards, turnaround and stop adjacent to the Ausgrid Tower to undertake maintenance works, and subsequently depart from the site in a forward direction.

The DA Drawings prepared by Millennium Design Consultants Pty Ltd provide details of the proposed works.

3.2 Proposed Use

The driveway and carpark will be utilised by staff of Al Noori Muslim School (primary and secondary campus staff). As such, the proposal involves a change of use from residential to educational establishment.

Additionally, as detailed above the proposed driveway, service areas and circulation space will also be utilised for access to, and maintenance of, the Ausgrid infrastructure as required by Ausgrid.

3.3 Associated Works

The proposed development includes works within the adjoining footpath and roadway of Greenacre Road. These works include:

- a new 6.8m wide crossover connecting the 5m hard stand to Greenacre Road;

- relocation of the existing road hump (and its associated barrier fencing and No Stopping signage) that fronts 81 Greenacre Road 5m to the east (to enable 6.8m wide crossover);
- demolition of the existing vehicle crossover for 81 Greenacre Road on the eastern side and reinstatement with standard kerb and gutter; and
- relocation of the existing drainage lintel fronting 81 Greenacre Road to the new road hump location.

The above works are detailed in the Letter of Advice regarding the proposed right of way prepared by TTPA and submitted with this DA under separate cover.

3.4 Landscaping and Fencing

A landscape plan has been prepared by A Total Concept Landscape Architects. The proposed landscaping works include proposed planting along the southern boundary which adjoins Greenacre Road with species selected having a mature height of up to 3m to enable appropriate sight distances for vehicles exiting the car park.

Fencing along the southern boundary (adjoining Greenacre Road) is proposed to consist of a brick fence with steel gates for entry and exit, consistent with the adjoining high school campus.

The landscape plan has provided a range of native species and endemic to the locality.

An extract of the landscape plan is provided at Figure 11 below.

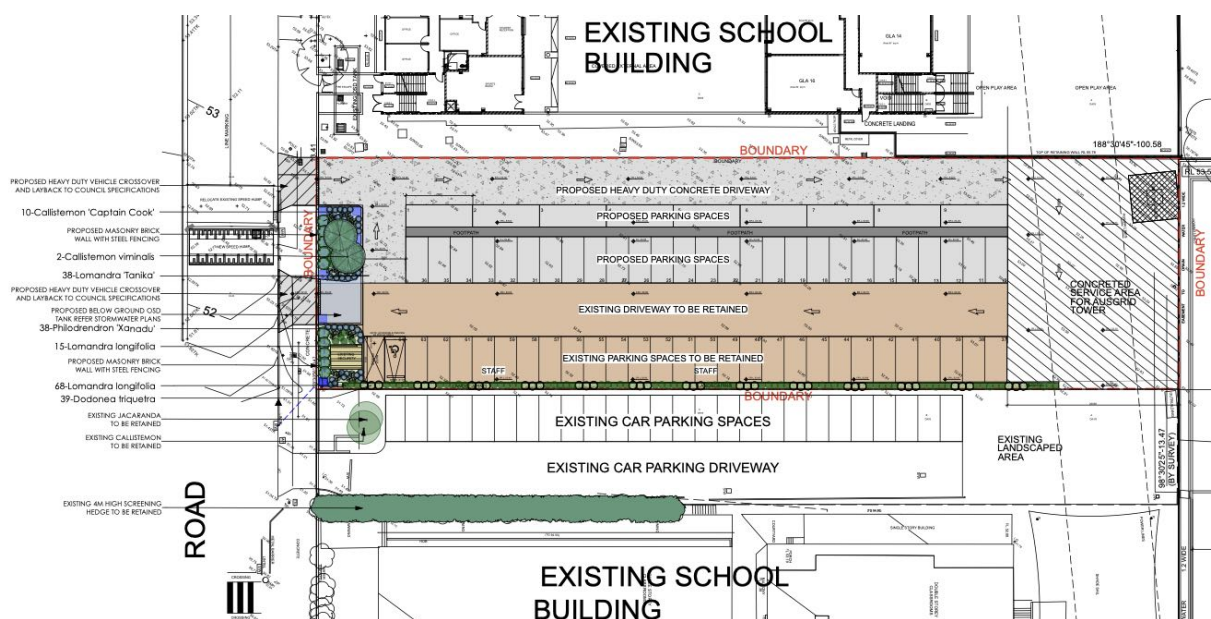


Figure 4: Extract of Landscape Plan

3.5 Stormwater

Stormwater concept plans have been prepared by Loka Consulting Engineers Pty Ltd. The stormwater concept includes the provision of on-site detention (OSD) to limit the post-development discharge to pre-development condition. The OSD tank is located along the southern boundary of the site adjoining Greenacre Road.

4 PLANNING ASSESSMENT

4.1 Overview

The matters for consideration under Section 4.15(1) of the *Environmental Planning and Assessment Act, 1979* (EP&A Act) that apply to the determination of development applications are detailed in Table 3 below.

Table 2 Section 4.15(1) Matters for consideration - general

Matter for consideration	Consideration
(a)(i) the provision of any environmental planning instrument	Consideration of the relevant EPIs is provided in Sections 4.2 – 4.4 below.
(a)(ii) any proposed instrument (that has been the subject of public consultation and has been notified to the consent authority	Consideration of any relevant Draft EPI is provided in Sections 4.5 below
(a)(iii) any development control plan	Consideration of the relevant DCP is provided at Section 4.6 below.
(a)(iiia) any planning agreement entered into under section 7.4, or any draft planning agreement that a developer has offered to enter into under section 7.4	Not applicable
(iv) the regulations	The application meets the relevant requirements of the EP& A Regulations
(b) the likely impacts of that development, including environmental impacts on both the natural and built environments, and social and economic impacts on the locality	The likely impacts of the development are considered at Section 5 .
(c) the suitability of the site for the development,	The site is suitable for the development (Sections 4 and 5)
(d) any submissions made in accordance with this Act or the regulations	Any submissions are considered at Section 5.5 below.
(e) the public interest	The proposal is in the public interest (Section 5.6).

4.2 State Environmental Planning Policy (Resilience and Hazards) 2021

State Environmental Planning Policy (Resilience and Hazards) 2021 commenced on 1 March 2022. Chapter 4 of the Resilience and Hazard SEPP relates to the remediation of land and aims to promote the remediation of contaminated land, for the purpose of reducing the risk of harm to human health or any other aspect of the environment, by specifying certain considerations that are relevant in determining development applications.

Clause 4.6 of the Resilience and Hazard SEPP requires that a consent authority must not consent to the carrying out of any development on land unless it has considered whether the land is contaminated, the suitability of the site to the proposed development and whether satisfactory measures are put into place to remediate the land should it be required to do so.

The subject site has been historically used for residential purposes, there is no information regarding its previous use that would indicate that the site is contaminated. Accordingly, there is nothing to warrant further investigation in relation to contamination and it is considered that the site is suitable for the proposed use as a car park and driveway.

4.3 State Environmental Planning Policy (Transport and Infrastructure) 2021

State Environmental Planning Policy (Transport and Infrastructure) 2021 commenced on 1 March 2022. Chapter 2 of the Transport and Infrastructure SEPP relates to Infrastructure and aims to facilitate the effective delivery of infrastructure across the state by improving regulatory certainty and efficiency, identifying matters to be considered in the assessment of development adjacent to particular types of infrastructure development, and providing for consultation with relevant public authorities about certain development during the assessment process.

Given the presence of the electricity transmission lines and an associated easement on the site, under Clause 2.48 of the Transport and Infrastructure SEPP council are required to give written notice to the electricity supply authority for the area (in this case Ausgrid) inviting comments about potential safety risks.

Chapter 3 of the Transport and Infrastructure SEPP relates to educational establishments and child care facilities and aims to facilitate the effective delivery of educational establishments and early education and care facilities across the State

Under Clause 3.36(1) of the Transport and Infrastructure SEPP, “schools” are permitted with development consent in a prescribed zone. The R2 Residential Low Density Residential zone (the relevant zoning of the site) is a prescribed zone and as such the proposed development is permissible with development consent.

Clause 3.36(6) requires:

(6) Before determining a development application for development of a kind referred to in subsection (1), (3) or (5), the consent authority must take into consideration—

(a) the design quality of the development when evaluated in accordance with the design quality principles set out in Schedule 8, and

(b) whether the development enables the use of school facilities (including recreational facilities) to be shared with the community.

An assessment of the development against the design quality principles is provided in Table 4.

Table 3 Design Quality Principles

Design Principle	Response
Principle 1—context, built form and landscape	<p>The proposed car park has been designed to ensure ease of access and efficient traffic flow, with clearly marked spaces, and pedestrian pathways, that enhance safety and usability.</p> <p>The provision of landscaping as part of the proposed works will enhance on-site amenity and soften the impact of the built form.</p>
Principle 2—sustainable, efficient and durable	The car park and driveway has been designed to ensure they are durable and adaptable.
Principle 3—accessible and inclusive	The proposal includes accessible parking adjoining the security building and the car park exit.
Principle 4—health and safety	<p>The existing fencing surrounding the development zone provides safety and security for staff. The fencing will be retained, and gates provided to enable pedestrian access.</p> <p>Clear and visible signage will be provided to direct traffic flow, identify parking spaces, and enhance the overall usability of the car park.</p>
Principle 5—amenity	The proposed car park will provide an improvement in the volume of onsite parking for school staff. The car park will provide ample parking spaces and clearly marked pedestrian pathways.
Principle 6—whole of life, flexible and adaptive	The proposed car park will be built with durable materials that require minimal maintenance over its lifespan. This includes the provision of proper drainage to prevent water damage.
Principle 7—aesthetics	Whilst the proposal does not include any built forms, the landscaping proposed as part of the proposed development will provide a positive contribution to the streetscape and the wider character of the neighbourhood.

In regard to Clause 3.36(6)(b), the proposal does not include use of school facilities by the community.

4.4 Canterbury-Bankstown Local Environmental Plan 2023 (CBLEP 2023)

4.4.1 Zoning and Objectives

The subject site is zoned R2 Low Density Residential under the provisions of the CBLEP 2023.

The objectives of the R2 Low Density zone are as follows:

- *To provide for the housing needs of the community within a low density residential environment.*
- *To enable other land uses that provide facilities or services to meet the day to day needs of residents.*
- *To allow for certain non-residential uses that are compatible with residential uses and do not adversely affect the living environment or amenity of the area.*
- *To ensure suitable landscaping in the low density residential environment.*
- *To minimise and manage traffic and parking impacts.*
- *To minimise conflict between land uses within this zone and land uses within adjoining zones.*
- *To promote a high standard of urban design and local amenity.*

The proposal is consistent with the above objectives in that:

- The proposal enhances the provision of off-street parking for the existing educational establishment which has a positive parking impact on the adjoining residential area;
- The use is compatible with the residential uses and includes mitigation measures to ensure the proposal does not adversely affect the living environment or amenity of the area.
- Suitable landscaping is provided as part of the proposed development which will enhance the site and its interface with the adjoining streetscapes.

4.4.2 Permissibility

Under the provisions of CBLEP 2023, educational establishments (which include schools) are not permitted in the R2 Low Density zone. However, as detailed in Section 4.3 above, the proposed development is permissible under the provisions of Clause 3.36(1) of the Transport and Infrastructure SEPP

4.4.3 Development Standards

The proposal has been assessed against the relevant provisions of the CBLEP 2023, as detailed in Table 5 below.

Table 4 CBLEP 2023 Compliance Table

Clause	Compliance	Comment
Part 2 Permitted or prohibited development		
CI 2.7 – Demolition requires development consent	Yes	As detailed in Section 2.2, demolition of the previous structures has previously taken place (circa 2014).
Part 4 Principal development standards		
CI 4.3 – Height of buildings	N/A	The proposed development does not include the erection of any buildings
CI 4.4 – Floor space ratio	N/A	
Part 5 Miscellaneous provisions		
CI 5.10 – Heritage Conservation	N/A	The subject site is not identified as a heritage item, within a heritage conservation area or on which an Aboriginal object is located or within an Aboriginal place of heritage significance
Part 6 Additional Local Provisions		
CI 6.1 Acid sulfate soils	N/A	The site is not identified as containing acid sulfate soils.
6.2 Earthworks	Yes	The proposed development involves site grading to provide a level driveway and car park. The proposed stormwater concept plans include a Sediment and Erosion Control Plan which will be employed during the works.
6.3 Stormwater and water sensitive urban design	Yes	As detailed in Section 3.3, stormwater concept plans have been prepared by Loka Consulting Engineers Pty Ltd. The stormwater concept includes the provision of on-site detention (OSD) to limit the post-development discharge to pre-development condition.
6.9 Essential services	Yes	The subject site is currently provided with essential services and includes additional stormwater drainage works.

4.5 Draft Environmental Planning Instruments

There are no draft planning instruments of relevance to the proposed development.

4.6 Canterbury - Bankstown Development Control Plan 2023 (CBDCP 2023)

The Canterbury – Bankstown Development Control Plan (CBDCP 2023) came into effect on 23 June 2023 and applies to land within the Canterbury-Bankstown Local Government Area. The relevant controls of the CBDCP 2023 are identified and assessed in **Appendix A**.

5 ASSESSMENT OF ENVIRONMENTAL EFFECTS

5.1 Impacts on the Built Environment

5.1.1 Bulk and Scale

The proposed development does not involve the erection of any structures. The proposed landscaping which includes planting along the boundaries of the site will soften the appearance of the hardscape and provide an aesthetically pleasing presentation of the carpark to the adjoining street.

5.1.2 Overshadowing

The proposed development does not involve the erection of any structures and will therefore not result in any overshadowing impacts.

5.1.3 Privacy Impacts

The proposed car park is adjoined by the existing Al Noori Muslim school to the north, east and west. The car park will be utilised during school operating hours and will be provided with security gates to prevent use outside these hours.

As a result, it is anticipated that there will be no adverse amenity impacts arising from the proposal in relation to privacy.

5.1.4 Visual and View Impacts

The proposed development will not have an adverse impact on visual or views to or across the site. Landscaping is proposed along the street frontage of Greenacre Road. Which consists of a mix of ground covers, shrubs, and trees. This will enhance the visual appearance of the site when viewed from the directly adjoining locality.

5.1.5 Acoustic Impacts

As detailed above, the proposed car park is adjoined by the existing Al Noori Muslim school to the north, east and west. Acoustic impacts are minimal and will not be perceptible above the existing background noise levels. The car park will be utilised during school operating hours and will be provided with security gates to prevent use outside these hours.

5.1.6 Traffic and Parking Impacts

The proposed development does not involve an intensification of the existing primary or secondary school regarding student population and will therefore not impact on traffic generation. The proposal will provide 55 parking spaces for staff of the primary and secondary school. As such the proposal will have a beneficial parking impact by reducing on-street parking demand for the adjoining streets.

5.1.7 Stormwater Impacts

As detailed in Section 3.5, the proposal includes the provision of on-site detention (OSD) to limit the post-development discharge to pre-development condition. The OSD tank is located along the southern boundary of the site adjoining Greenacre Road.

It is anticipated there will be no adverse stormwater impacts as a result of the proposal.

5.2 Impacts on the Natural Environment

The development zone has previously been utilised for residential purposes. The proposal will have no significant impact on the natural environment as much of the site has been previously cleared.

The proposal includes new plantings including trees, shrubs, ground covers and grass as part of the overall landscape scheme. This will enhance the natural environment of the site and the locality.

5.3 Social and Economic Impacts

The proposed addition of car parking will provide staff with parking which adjoins the existing school campus. It is noted that the proposed use of the site as part of the school does not involve an increase in student numbers for the existing primary or secondary school.

The proposed car park will be utilised during school hours.

5.4 The Suitability of the Site

The site is entirely suitable for the proposed car park which is to be used as part of the existing school. The additional parking spaces will be utilised by staff of the primary and secondary school during school hours.

The site is provided with the necessary services and facilities and the existing Al Noori school is well established and connected to pedestrian and public transport networks.

The proposed landscaping works will provide additional soft landscaping along the boundaries of the development zone and at its interface with Greenacre Road.

5.5 Submissions

Any relevant submissions will need to be considered by the consent authority in the determination of the DA.

5.6 Public Interest

The proposal is in the public interest as it will provide additional onsite parking for the staff of Al Noori Muslim primary and secondary school. The proposed works are minimal with the works limited to minor grading, concrete pouring, stormwater works and landscape works.

6 CONCLUSION

The proposed development is for the addition of car parking spaces for the staff of Al Noori Muslim primary and secondary school and the construction of a heavy duty driveway for access by Ausgrid to their infrastructure located at the rear of the site. The proposed development satisfies the relevant planning controls relating to the site and proposal.

The proposed use as part of the existing primary and secondary school will enhance the amount of off street car parking for the existing staff population.

The proposal will be compatible with the immediate surrounding locality and will not impact on the amenity of surrounding dwellings.

Having regard to the above and in light of the matters for consideration listed in Section 4.15(1) of the EP &A Act 1979, the proposal is considered to be in the public interest and warrants favourable consideration by Council.

APPENDIX A

CANTERBURY- BANKSTOWN DCP
2023

Table 5 Compliance Table – Canterbury-Bankstown DCP 2023

Section	Compliance	Comment
Chapter 3 – General Requirements – 3.2 Parking		
Section 3 Design and Layout		
Parking Location 3.1 Development must not locate entries to car parking or delivery areas: (a) close to intersections and signalised junctions; (b) on crests or curves; (c) where adequate sight distance is not available; (d) opposite parking entries of other buildings that generate a large amount of traffic (unless separated by a raised median island); (e) where right turning traffic entering may obstruct through traffic; (f) where vehicles entering might interfere with operations of bus stops, taxi ranks, loading zones or pedestrian crossings; or (g) where there are obstructions which may prevent drivers from having a clear view of pedestrians and vehicles.	Yes	The entry and exit to the car park is located away from intersections, crests and other obstructions. The proposal triggers the requirement for the relocation of the existing road hump (and its associated barrier fencing and No Stopping signage) that fronts 81 Greenacre Road 5m to the east (to enable 6.8m wide crossover).
3.2 Parking areas for people with disabilities should be close to an entrance to development. Access from the parking area to the development should be by ramps or lifts where there are separate levels.	Yes	The accessible parking space is located near the car park exit where direct access to the footpath is available.

Section	Compliance	Comment
3.3 Where above ground parking is the only solution possible, locate to the rear of buildings.	Merit assessment	The proposed at-grade parking area utilises an area of the school which is currently vacant (with exception to minor structures). The proposal includes landscaping of the area between the 2 driveways and fronting Greenacre Road which assists in providing a visual buffer to the parking area.
Access driveway width and design 3.8 The location of driveways to properties should allow the shortest, most direct access over the nature strip from the road.	Yes	Refer to submitted DA Drawings.
3.9 The appropriate driveway width is dependent on the type of parking facility, whether entry and exit points are combined or separate, the frontage road type and the number of parking spaces served by the access facility.	Yes	Refer to traffic letter prepared by TTPA.
3.10 Driveway widths for existing dwellings and extensions to the existing properties are assessed on their merits.	N/A	
Sight distance requirement 3.19 For all development, adequate sight distance must be provided for vehicles exiting driveways. Clear sight lines are to be provided at the street boundary to ensure adequate visibility between vehicles on the driveway and pedestrians on the footway and vehicles on the roadway.	Yes	The proposed egress driveway is provided with clear sight lines at the street boundary.
Pedestrian access 3.20 Parking areas should be designed so that through-traffic is excluded, and pedestrian entrances and exits are separate from vehicular entrances and exits.	General compliance	The parking area will not be utilised for through traffic (staff only car park). Separate pedestrian pathway is provided in the car park with shared entry and exit with vehicles which is deemed to be appropriate given the turnover of parking by staff.

Section	Compliance	Comment
3.21 Lifts and stair lobbies should be prominently marked to help users find them and to increase personal security.	N/A	
3.22 In split-level/multi-level car parks, a stairway should be located at the split-level, to provide pedestrian access between these levels and eliminate pedestrians having to use vehicular ramps.	N/A	
At-grade parking 3.42 Screen or enclose at-grade parking with landscaping, structures or by wrapping the car park with retail or other active uses.	Yes	The proposal includes landscaping along the frontage to Greenacre Road.
3.43 Avoid car parking areas and access driveways characterised by large expanse of bare concrete.	General compliance	The proposed development includes the provision of a heavy duty driveway and access area for Ausgrid to service and maintain their infrastructure. As such the provision of bare concrete is unavoidable. Notwithstanding, the proposal provides landscaping within the front setback which will soften the hardscaping.
3.44 Use a combination of different surface materials to delineate pedestrian thoroughfares, vehicular access and parking areas.	Yes	Different surfaces can be utilised to delineate between the pedestrian footpath and parking area.
3.45 Use perforated paving materials (for example, paving units with wide bands of gravel aggregates) that allow infiltration of stormwater.	Merit assessment	The proposed development includes the provision of a heavy duty driveway for Ausgrid which is required to be constructed to accommodate large and heavy vehicles. Stormwater will be directed to the proposed OSD at the front of the site.
3.46 Trees are to be planted at the ratio of one tree per five car park places allocated. Species are to be selected for their ability to		Due to the access requirements of Ausgrid to their infrastructure the provision of trees within the driveway and carpark areas is restricted.

Section	Compliance	Comment
thrive where compaction and deoxygenation are characteristic of the soils.	Merit assessment	Notwithstanding, the planting of shrubs and ground covers along the Greenacre Road will assist with the softening of the hardscape area when viewed from Greenacre Road.
3.47 For proposed car parks of capacity 40 cars or more, raised landscape island beds of minimum dimensions 2m by 4m shall be provided to break up row of cars, spaced at every ten car places for placement of a canopy tree.		
Chapter 10 – Other Development – Section 10.2 - Schools		
Section 3 – Location and Traffic Management		
3.1 Traffic management (environmental capacity) Development for the purpose of schools must not result in a street in the vicinity of the site to exceed the environmental capacity maximum. If the environmental capacity maximum is already exceeded, the development must maintain the existing level of absolute delay of that street.	Yes	The proposed development does not result in an intensification of the current school in regard to student population and will therefore not impact on traffic generation. The proposal provides an additional 64 parking spaces and hence the proposal involves a reduction in the use of on-street parking.
3.2 Traffic management (level of service) Development for the purpose of schools must not result in a street intersection in the vicinity of the site to have a level of service below Level B. If the existing level of service is below Level B, the development must maintain the existing level of absolute delay of that street intersection.	Yes	See comment above.
3.3 Traffic impact studies	N/A	A Traffic Impact Study is not warranted for the proposed development given the proposal does not intensify the existing school use.

Section	Compliance	Comment
Section 4 – Site Layout and Building Envelopes		
<p>4.1 Site width in residential zones</p> <p>Development for the purpose of schools within Zone R2 Low Density Residential, Zone R3 Medium Density Residential or Zone R4 High Density Residential must ensure the site is at least 40m wide at the front building line.</p>	Yes	The development zone has a frontage to Greenacre Road of approximately 26m and the subject site has a frontage of 120m.
<p>4.8 Setbacks</p> <p>The minimum setback for schools (including car parks and basements) to the primary and secondary street frontages in Zone R2 Low Density Residential is:</p> <p>primary street frontage - 9m (or a distance equal to the proposed maximum building height, whichever is the greater)</p> <p>secondary street frontage - 6m (or a distance equal to the proposed maximum building height, whichever is the greater)</p>	General compliance	The proposed car parking spaces are setback between 9.4m and 5.4m from Greenacre Road. The proposed setbacks are considered to be appropriate given that a sufficient area of landscaping is provided between the entry and exit driveways which will soften the appearance of the hardscape areas and the proposed setbacks are consistent with the setbacks of the adjoining school buildings and adjoining existing car park fronting Greenacre Road.
<p>4.12 Deep Soil Zones</p> <p>Development for the purpose of schools within Zone R2 Low Density Residential must provide deep soil zones that have the following minimum widths around the boundary of the site:</p> <p>primary street frontage - 9m</p> <p>secondary street frontage - 6m</p> <p>side and rear setbacks – 5m</p> <p>The deep soil zones must be landscaped by way of deep soil plantings and canopy trees.</p>	General compliance	<p>The proposed development includes a deep soil zone fronting Greenacre Road and along the eastern boundary of the development zone. Within the deep soil zones plantings are to include canopy trees, shrubs, and ground covers. Given the proposed use of this area (i.e. no buildings are proposed) the proposed landscaping is considered to be an appropriate design outcome and meets the objectives of the development control.</p> <p>The proposed landscaping is provided between the entry and exit driveways which will soften the appearance of the hardscape areas. The proposed landscape treatment and setbacks are consistent with</p>

Section	Compliance	Comment
		the setbacks of the adjoining school buildings and adjoining existing car park fronting Greenacre Road.
<p>4.14 Free Play areas</p> <p>Development for the purpose of primary schools must dedicate at least 12m² of site area per student for the exclusive use of free play areas. The minimum size of the free play areas must equate to the greatest number of students that could use the free play areas at any one time. The free play areas must locate at ground level. In this clause, free play areas mean outdoor useable spaces and playing fields that are for the use of students for physical activities and team games.</p>	N/A	No change
<p>4.16 Free Play areas</p> <p>Schools must ensure the location of outdoor areas and free play areas avoids:</p> <p>(a) Existing native vegetation including under storey native vegetation.</p> <p>(b) Potential traffic hazard locations where an out-of-control vehicle may injure students.</p>	N/A	No change
<p>4.17 Access</p> <p>Schools must be easily accessible to people with disabilities and must comply with the Building Code of Australia and Australian Standard AS 1428.1–2021, Design for access and mobility.</p>	Yes	The proposal includes disabled parking located near the egress driveway which enables direct access to the footpath and onto the school buildings to the east and west.
4.18 Access	No change	The proposal will not impact on existing emergency vehicle access.

Section	Compliance	Comment
Provision must be made for access and parking of service and emergency vehicles to service all buildings within the school.		
Section 5 – Energy Efficiency and Urban Design		
5.3 Access to sunlight The design of buildings must ensure there is adequate solar access to the free play areas.	N/A	N/A
5.8 Front Fences The maximum fence height for front fences is 1.8m.	Yes	Fencing along Greenacre Road will consists of a brick fence with steel gates for entry and exit, consistent with the adjoining high school campus.
5.9 Front Fences The external appearance of a front fence along the front boundary of the site must ensure: (a) the section of the front fence that comprises solid construction (not including pillars) does not exceed a fence height of 1m above ground level (existing); and (b) the remaining height of the front fence comprises open style construction such as spaced timber pickets or wrought iron that enhance and unify the building design.		
5.10 Front Fences Council does not allow the following types of front fences: (a) chain wire, metal sheeting, brushwood, and electric fences; and (b) noise attenuation walls.		

Section	Compliance	Comment
Section 6 – Acoustic Privacy and Management		
<p>6.2 Acoustic Privacy</p> <p>The location and design of schools must consider the projection of noise from various activities to avoid any adverse impacts on the residential amenity of adjoining land. For the purpose of this clause, Council requires development applications to submit an Acoustic Report prepared by a suitably qualified acoustic consultant to determine:</p> <ul style="list-style-type: none"> (a) existing noise levels at the identified sensitive receiver locations; (b) likely noise levels to emanate from the school at the identified sensitive receiver locations; (c) whether the development must apply measures to ensure the noise of students does not exceed 10dB(A) above the background noise level; (d) whether the location and setbacks of the development are sufficient to protect the acoustic privacy of adjacent dwellings; (e) whether the location of the outdoor areas and free play areas should avoid living areas and bedrooms of adjacent dwellings; and (f) whether the development must install certain noise attenuation measures to protect the acoustic privacy of adjacent dwellings. <p>The Acoustic Report must measure the noise readings over a 15 minute period and must provide details of all modelling assumptions including source noise data, noise monitoring positions, receiver heights and locations, prevailing meteorological</p>	Yes	<p>The proposed car park is adjoined by the existing Al Noori Muslim school to the north, east and west. Acoustic impacts are minimal and will not be perceptible above the existing background noise levels. The car park will be utilised during school operating hours and will be provided with security gates to prevent use outside these hours.</p>

Section	Compliance	Comment
conditions during the monitoring, confirmation of the methodology adopted along with a copy of the model input and output data.		
6.3 Acoustic Privacy The maximum height for noise attenuation walls and fences along the boundary of the site is 2m.	N/A	
6.4 Hours of operation Council may limit the hours of operation of schools, public access to schools, and special occasions or events.	No change	The proposed development does not seek to modify the existing school hours.
Section 7 - Landscape		
7.1 Landscaping Development applications must submit a detailed landscape plan prepared by a qualified landscape architect consistent with Council's Landscape Guide.	Yes	Landscape Plans have been prepared by A Total Concept Landscape Architects to accompany the development application.
7.2 Landscaping Trees and shrubs that require low maintenance should be of prime consideration in the choice of planting. Features such as mulched garden beds, use of perennial rather than annual plants and mowing strips reduce the need for maintenance.	Yes	The proposed landscaping scheme includes native shrubs which require low maintenance. The planter beds will be mulched.
Section 8 – Safety and security		
8.1 Safety and security	Yes	The proposed development retains the existing security building fronting Greenacre Road which will be staffed during school hours.

Section	Compliance	Comment
Development for the purpose of schools must comply with the Crime Prevention through Environmental Design Policy in consultation with Council and NSW Police.		
8.2 Safety and security Development for the purpose of schools must provide active frontages to the streets and must orientate buildings, administration buildings and pedestrian entrances to the streets.	Yes	The proposed car park will provide an active frontage during school hours.
8.3 Safety and security The street number of schools must be visible from the street and made of a reflective material to allow visitors and emergency vehicles to easily identify the location of schools.	Can comply	Additional street numbers for the existing school can be provided if required by council (i.e. as a condition of consent).
8.4 Safety and security Development for the purpose of new schools must submit a Social Impact Assessment to the satisfaction of Council.	N/A	The proposed development does not propose a new school.